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Freeborn & Peters

June 10, 2002

VIA FEDERAL EXPRESS

Mr. Vernon A. Williams
Secretary
Surface Transportation Board
1925 K Street, N.W.
Washington, DC 20423-0001

DID # 0101966001



**Re: Surface Transportation Board Docket No. AB-6 (Sub-No. 394X);
The Burlington Northern and Santa Fe Railway Company Abandonment
between Powers Lake, North Dakota to Grenora, North Dakota**

Attorneys at Law

311 South Wacker Drive
Suite 3000
Chicago, Illinois
60606-6677
Tel 312.360.6000

Brian Nettles
Paralegal
Direct 312.360.6336
Fax 312.360.6596
bnettl@
freebornpeters.com

Dear Secretary Williams:

Enclosed for filing in the referenced docket are the original and ten copies of The Burlington Northern and Santa Fe Railway Company's Notice of Exemption, and a check for \$2,700.00, covering the filing fee.

Please acknowledge receipt of this material by date stamping the enclosed copy of this letter and returning it to me in the enclosed self-addressed stamped envelope.

The environmental and historic reports have been forwarded to the responsible agencies and copies have been directed to the Section of Environmental Analysis. The Notice of Exemption includes a certificate of notice upon various interested state and federal authorities (the U. S. Department of the Interior – National Park Service, Recreation and Conservation Assistance Program; the U. S. Department of the Interior – National Park Service, Land Resource Division; the Department of Defense, Military Traffic Management Command; the North Dakota Department of Transportation; the North Dakota Public Service Commission and the Chief of Forest Service, U. S. Department of Agriculture).

Sincerely,

Brian Nettles

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SURFACE
TRANSPORTATION BOARD

/bn
Enclosures

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Public Record

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SURFACE
TRANSPORTATION BOARD

Chicago

Springfield

205587

BEFORE THE
SURFACE TRANSPORTATION BOARD

Abandonment of a line of Railroad
Between BNSF M.P. 26.59 in Powers,
Lake, North Dakota and M.P. 87.10
in Grenora, North Dakota

NOTICE OF EXEMPTION
Docket No. AB-6
(Sub-No. 394X)



TO THE SURFACE TRANSPORTATION BOARD:

The Burlington Northern and Santa Fe Railway Company (BNSF) hereby notifies the Board that it intends to abandon its line of railroad between M.P. 26.59 in Powers Lake, Burke County, North Dakota and M.P. 87.10 in Grenora, Williams County, North Dakota, total distance of 60.51 miles, pursuant to the Board's Exemption of Out of Service Rail Lines, 49 C.F.R. § 1152.50 and in support of the Notice, respectfully provides the following information:

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SURFACE
TRANSPORTATION BOARD

- I. Proposed consummation date for the abandonment.

July 31, 2002

- II. Certification required in 49 C.F.R. § 1152.50(b)

The attached Certification of Michael Smith certifies that no formal complaint filed by a user of rail service on this line (or a state or local government entity acting on behalf of such user) regarding cessation of service over the line is pending with the Board or any U.S. District Court or has been decided in favor of a complainant within the last two years. The attached Verification of Richard A. Batie certifies that no local traffic has moved over this line for at least two (2) years and any overhead traffic on the line can be rerouted over other lines.

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III. Information required by C.F.R. § 1152.22(a)(1-4), (7), (8) and (e)(4).

(a) General

- (1) Exact name of Applicant.

The Burlington Northern and Santa Fe Railway
Company.

- (2) Whether applicant is a common carrier by railroad subject
to 49 U.S.C. Subtitle IV, Chapter 105.

Applicant is a common carrier by railroad subject to
49 U.S.C. Subtitle IV, Chapter 105.

- (3) Relief sought.

BNSF seeks an exemption from Surface Transportation
Board approval authorizing the abandonment and
discontinuance of service of the Powers Lake, North
Dakota to Grenora, North Dakota line specified above.

- (4) Detailed map of the subject line.

See Exhibit A.

- (5) Name, title and address of representative of applicant to
whom correspondence should be sent.

Michael Smith
Freeborn & Peters
311 S. Wacker Dr., Suite 3000
Chicago, Illinois 60606-6677

- (6) List of all United State Postal Service ZIP Codes that the
line traverses.

The Powers Lake, North Dakota to Grenora, North Dakota line traverses United States Postal Service Zip Codes 58845, 58856, 58830, 58795, 58755, and 58773.

- (7) Statement of whether the properties proposed to be abandoned are suitable for other public purposes, including roads or highways, other forms of mass transportation, conservation, energy production or transmission or recreation. If the applicant is aware of any restriction on the title to the property, including any reversionary interest, which would affect the transfer of title or the use of property for other than rail purposes, this shall be disclosed.

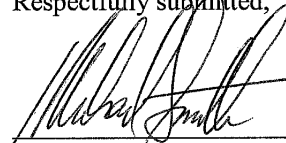
The right of way may be suitable for use for other public purposes such as a trail. On part of the properties involved, title considerations may affect the conveyance of the land for use other than railroad purposes. Based on information in our possession, the line contains federally granted rights-of-way on three parcels. Any documentation in the railroad's possession will be made available promptly to those requesting it.

IV. Level of protection.

The interest of railroad employees will be protected by the conditions imposed in Oregon Short Line RR Co. - Abandonment - Goshen, 360 I.C.C. 91 (1979)

V. Certification that the notice requirements of §§ 1152.50(d)(1) and 1105.11 have been met. See attached.

Respectfully submitted,

A handwritten signature in dark ink, appearing to read "Michael Smith", is written over a horizontal line.

Michael Smith
Freeborn & Peters
311 S. Wacker Dr., Suite 3000
Chicago, Illinois 60606-6677
(312) 360-6724
FAX (312) 360-6598

Date: June 10, 2002

VERIFICATION

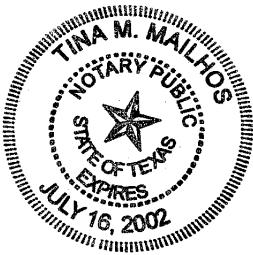
STATE OF TEXAS)
)ss
COUNTY OF TARRANT)

Richard A. Batie makes oath and says he is Manager, Shortline Development in the Network Development department of The Burlington Northern and Santa Fe Railway Company; that he has been authorized by the Applicant to verify and file with the Surface Transportation Board the foregoing Notice of Exemption in AB-6 (Sub-No. 394X); that he affirms that the line has had no local traffic in the prior two (2) years and that there is no overhead traffic to be rerouted; that he has carefully examined all of the statements in the Notice; that he has knowledge of the facts and matters relied upon in the Notice; and that all representations set forth therein are true and correct to the best of his knowledge, information, and belief.



Richard A. Batie
Manager, Shortline Development

Subscribed and sworn to before me the 4th day of June, 2002.

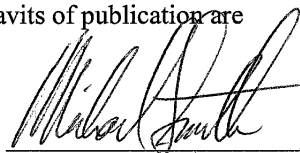



Notary Public

THE BURLINGTON NORTHERN AND SANTA FE RAILWAY COMPANY
NO. AB-6 (Sub No. 394X)

CERTIFICATION

I hereby certify that (1) no formal complaint filed by a user of rail service on this line (or a state or local government entity acting on behalf of such user) regarding cessation of service over the line is pending with the Board or any U.S. District Court or has been decided in favor of a complainant within the last two (2) years; (2) service of notice upon the U.S. Department of the Interior, National Park Service, Recreation Park Service, Land Resource Division; the Department of Defense, Military Traffic Management Command; the North Dakota Public Service Commission; the North Dakota Department of Transportation; and the Chief Forest Service, U.S. Department of Agriculture was accomplished by mailing a copy thereof on May 10, 2002; (3) the environmental and historic reports were serviced upon the agencies identified in 49 C.F.R. § 1105.7(b) and the State Historic Preservation Offices on May 10, 2002; (4) service of notice upon the State Clearinghouses was accomplished by mailing a copy thereof on May 10, 2002; and (5) the publication requirements of 49 C.F.R. § 1105.12 have been complied with, to wit: publication of notice was accomplished on May 20, 2002 in the Williston Daily Herald and on May 22, 2002 in the Burke County Tribune, (affidavits of publication are attached).



Michael Smith
Freeborn & Peters
311 S. Wacker Dr., Suite 3000
Chicago, Illinois 60606-6677
(312) 360-6724
FAX (312) 360-6598

Date: June 10, 2002

NOTICE OF INTENT TO ABANDON

The Burlington Northern and Santa Fe Railway Company gives notice that on or after June 3, 2002, it intends to file with the Surface Transportation Board, Washington, DC 20423, an abandonment notice of exemption under 49 CFR 1152 Subpart F - Exempt Abandonments permitting the abandonment of or discontinuance of service on 87.10 miles of railroad line between railroad milepost 26.59 near Powers Lake and milepost 87.10 in Grenora, which line segment traverses through United States Postal Zip Codes 58845, 58856, 58830, 58795, 58755, and 58773 in Burke and Williams County, North Dakota. The proceeding will be docketed as No. AB-6 (Sub-No. 394X).

The Board's Section of Environmental Analyses (SEA) will generally prepare an Environmental Assessment (EA), which will normally be available 25 days after the filing of the notice of exemption. Comments on environmental and energy matters should be filed no later than 15 days after the EA becomes available to the public and will be addressed in a Board decision. Interested persons may obtain a copy of the EA or make inquiries regarding environmental matters by writing to the Section of Environmental Analysis, Surface Transportation Board, 2925 K Street, N.W., Washington, DC 20423-0001 or by calling that office at 202-565-1538.

Appropriate offers of financial assistance to continue rail service can be filed with the Board. Requests for environmental conditions, public use conditions, or rail banking/trails use also can be filed with the Board. An original and 10 copies of any pleading that raises matters other than environmental issues (such as trails use, public use, and offers of financial assistance) must be filed directly with the Board's Office of the Secretary, 1925 K Street, N.W., Washington DC 20423-0001 [See 49 CFR 1104.1(a) and 1104.3(a)], and one copy must be served on applicants' representative [See 49 CFR 1104.12(a)]. Questions regarding offers of financial assistance, public use or trails use may be directed to the Board's Office of Public Services at 202-565-1592. Copies of any comments or requests for conditions should be served on the applicant's representative Michael Smith, Freeborn & Peters, 311 S. Wacker Drive, Suite 3000, Chicago, Illinois 60606-6677, telephone (312) 360-6724.

(May 15, 2002)

AFFIDAVIT OF PUBLICATION

STATE OF NORTH DAKOTA }
County of Williams }

ss.

8391

I, LeAnn K. Volz being first duly sworn, on my oath, say that I am the bookkeeper of The Williston Daily Herald, a daily newspaper of general circulation, published in the city of Williston, in said county and state, and that the advertisement headed:

NOTICE OF INTENT TO ABANDON

a printed copy of which is here annexed, was published in the regular and entire issue of said newspaper during the period and time of publication, and that the notice was published in the newspaper proper, and not in a supplement.

for 1 consecutive week/weeks to wit:

Straight Matter Lines 60 May 15, 2002

First Time Line Rate .66

Subsequent Line Rates .66

Column Inches _____

First Time Inch Rate 5.00

Subsequent Inch Rates 5.00

Total Cost of Legal, \$ 39.60

Subscribed and sworn to before me this 20th day of

May

A.D. 2002

(Seal) Beverly J. Forthum

Notary Public, State of North Dakota

BEVERLY J. FORTHUM
Notary Public, State of North Dakota
My Commission Expires Sept. 28, 2004

THE BURKE COUNTY TRIBUNE
AFFIDAVIT OF PUBLICATION

STATE OF NORTH DAKOTA)
COUNTY OF BURKE)

Kristi M. Bohl of said County and State, being first duly sworn, on her oath, says that she is the owner of The Burke County Tribune, a weekly newspaper of general circulation, printed at Bowbells, in said County and State, and that she has been during the time hereinafter mentioned, and that the Notice of Publication of

Notice of Intent
to Abandon

a printed copy of which is hereto annexed, was published in the regular and entire issue of said newspaper during the period and time of publication, and that the Notice was published in the newspaper proper and not in supplement, once each week for 1 successive weeks, to wit:

522 2002 20 20

20 20 20

once each week for successive weeks
and published in the newspaper proper and not in supplement, and that the Notice

Kristi M. Bohl, Publisher

Subscribed and sworn to before me this 22nd

May, 2002

Lynn Rick

LYNN RICK
Notary Public in and for
Notary Public, BURKE COUNTY, N. DAK. Burke County, North Dakota
My Commission Expires, AUG. 6, 2002

1st time 62 lines at 49 per line 30.38

2nd time 20 lines at 20 per line 400

3rd time 20 lines at 20 per line 400

1st time 20 column inches at 20 per line 400

2nd time 20 column inches at 20 per line 400

TOTAL 1100

Notice of Intent
to Abandon

The Burlington Northern and Santa Fe Railway Company gives notice that on or after June 3, 2002, it intends to file with the Surface Transportation Board, Washington, DC 20423, an abandonment notice of exemption under 49 CFR 1152 Subpart F -- Exempt Abandonments permitting the abandonment of or discontinuance of service of 87.10 miles of railroad line between milepost 26.59 near Powers Lake and milepost 87.10 in Grenora, which line segment traverses through United States Postal Zip Codes 58845, 58856, 58830, 58795, 58755, and 58773 in Burke and Williams County, North Dakota. The proceeding will be docketed as No. AB-6 (Sub-No. 394X).

The Board's Section of Environmental Analyses (SEA) will generally prepare an Environmental Assessment (EA), which will normally be available 25 days after the filing of the notice of exemption. Comments on environmental and energy matters should be filed no later than 15 days after the EA becomes available to the public and will be addressed in a Board decision. Interested persons may obtain a copy of the EA or make inquiries regarding environmental matters by writing to the Section of Environmental Analysis, Surface Transportation Board, 1925 K Street, N.W., Washington, DC 20423-0001 or by calling that office at 202-565-1538.

Appropriate offers of financial assistance to continue rail service can be filed with the Board. Requests for environmental conditions, public use conditions, or rail banking/trails use also can be filed with the Board. An original and 10 copies of any pleading that raises matters other than environmental issues (such as trails use, public use, and offers of financial assistance) must be filed directly with the Board's Office of the Secretary, 1925 K Street, N.W., Washington DC 20423-0001 [See 49 CFR 1104.1(a) and 1104.3(a)], and one copy must be served on applicants' representative [See 49 CFR 1104.12(a)]. Questions regarding offers of financial assistance, public use or trails use may be directed to the Board's Office of Public Services at 202-565-1592. Copies of any comments or requests for conditions should be served on the applicant's representative Michael Smith, Freeborn & Peters, 311 S. Wacker Drive, Suite 3000, Chicago, Illinois 60606-6677 telephone (312) 360-6724.

May 22, 2002

BEFORE THE
SURFACE TRANSPORTATION BOARD

In the Matter of The
Burlington Northern and Santa Fe
Railway Company
Petition for Exemption to Abandon
Its Line of Railroad Between M.P.
26.59 in Powers Lake, North Dakota and
M.P. 87.10 in Grenora, North Dakota

Docket No. AB-6
(Sub No. 394X)

ENVIRONMENTAL REPORT

The following information is submitted to the Surface Transportation Board by The Burlington Northern and Santa Fe Railway Company (BNSF) in accordance with the Board's reporting requirements as set forth in 49 C.F.R. § 1105.7 for the purpose of assisting the Board's preparation of an environmental document regarding BNSF's Notice of Exemption for abandonment of its line between M.P. 26.59 in Powers Lake, Burke County, North Dakota and M.P. 87.10 in Williams County, North Dakota, a total distance of 60.51 miles.

(1) Proposed action and Alternatives: Describe the proposed action, including commodities transported, the planned disposition (if any) of any rail line and other structures that may be involved, and any possible changes in current operations or maintenance practices. Also describe any reasonable alternatives to the proposed action. Include a readable, detailed map and drawings clearly delineating the project.

BNSF seeks to abandon and discontinue service of the line. A map of the line is attached as Exhibit A.

(2) Transportation system: Describe the effect of the proposed action on regional or local transportation systems and patterns. Estimate the amount of traffic (passenger or freight) that will be diverted to other transportation systems or modes as a result of the proposed action.

The proposed abandonment will have no affect on existing transportation systems or patterns as the line is out of service.

(3) Land Use:

(i) Based on consultation with local and/or regional planning agencies and/or review of the official planning documents prepared by such agencies, state whether the proposed action is consistent with existing land use plans. Describe any inconsistencies.

The proposed abandonment should be consistent with local land use plans. The Board of Commissioners in Burke and Williams County have been notified and have not responded as of the date of this report. See Exhibits B and C.

(ii) Based on consultation with the U.S. Soil Conservation Service, state the effect of the proposed action on any prime agriculture land.

The proposed abandonment will have no effect on prime agricultural land. See Exhibit D, response from the USDA Natural Resources Conservation Service.

(iii) If any action affects land or water uses within a designated coastal zone, include the coastal zone information required by § 1105.9

N/A

(iv) If the proposed action is abandonment, state whether or not the right-of-way is suitable for alternative public use under 49 U.S.C. § 10906 and explain why.

The proposed abandonment could be used for an alternative public use. The United States Department of the Interior, Fish and Wildlife Service is interested in purchasing portions of the right of way. See Exhibit E, letter from the United States Department of the Interior, Fish and Wildlife Service.

(4) Energy:

- (i) Describe the effect of the proposed action on transportation of energy.

To the best of BNSF's knowledge there are no undeveloped energy resources such as oil, natural gas or coal in the vicinity of this line.

- (ii) Describe the effect of the proposed action on recyclable commodities.

This abandonment and discontinuance will not adversely affect movement or recovery of recyclable commodities as the line is out of service.

- (iii) State whether the proposed action will result in an increase or decrease in overall energy efficiency and explain why.

This abandonment will not result in an increase or decrease in overall energy efficiency as the line is out of service.

- (iv) If the proposed action will cause diversions from rail to motor carriage of more than:

(A) 1,000 rail carloads a year, or

(B) An average of 50 rail carloads per mile per year for any part of the affected line, quantify the resulting net change in the energy consumption and show the data and methodology used to arrive at the figure given.

There will be no diversion of traffic because the line is out of service.

(5) Air:

- (i) If the proposed action will result in either:

(A). An increase in rail traffic of at least 100% (measured in gross ton miles annually) or an increase of at least eight trains a day on any segment of the line effected by the proposed, or

No.

(B). An increase in rail yard activity of at least 100% (measured by carload activity), or

No.

(C). An average increase in truck traffic of more than 10% of the average daily traffic or 50 vehicles a day on any affected road segment, quantify the anticipated effect on air emissions.

The action will not involve an increase in truck traffic of more than 10% or 50 vehicles per day on any affected road segment as the line is out of service.

(ii) If the proposed action affects a Class I or nonattainment area under the Clean Air Act, and will result in either:

(A) An increase in rail traffic of at least 50% (measured in gross ton miles annually) or an increase of at least three train a day on any segment of rail line, or

(B) An increase in rail yard activity of at least 20% (measured by carload activity), or

(C) An average increase in truck traffic or more than 10-% of the average daily traffic of 50 vehicles a day on a given road segment, then state whether any expected increased emissions are within the parameters established by State implementation Plan. However, for a rail construction under 49 U.S.C. § 10901 (or 49 U.S.C. § 10505) or in a case involving the reinstitution of service over a previously abandoned line, only the three train a day threshold in this item shall apply.

The proposed abandonment will not result in an increase of rail or truck traffic because the line is out of service.

(iii) If the transportation of ozone depleting materials (such as nitrogen oxide and from) is contemplated, identify: the materials and quantity; the frequency of service; safety practices (including any speed restrictions); the applicant's safety record (to the extent available) on derailments, accidents and spills; contingency plans to deal with accidental spills; and the likelihood of an accidental release of ozone depleting materials in the event of a collision or derailment.

This action will have no effect on the transportation of ozone-depleting materials as the line is out of service.

(6) Noise: If any of the thresholds identified in item (5)(c) of this section are surpassed, state whether the proposed action will cause:

(i) An incremental increase in noise levels of three decibels Ldn or more,

N/A

(ii) An increase to a noise level of 76 decibels Ldn or greater. If so, identify sensitive receptors (e.g. schools, libraries, hospitals, residences, retirement communities and nursing homes) in the project area and quantify the noise increase for those receptors if the thresholds are surpassed.

N/A

(7) Safety:

(i) Describe any effects of the proposed action on public health and safety (including vehicle delay time at railroad crossings).

This abandonment should have no adverse effect on health or public safety. There are 64 public crossings and 35 private crossings.

During salvage operations on the line, precautions will be taken to ensure public safety, and contractors will be required to satisfy all applicable health and safety laws and regulations.

(ii) If hazardous materials are expected to be transported, identify: the materials and quantity; the frequency of service; whether chemicals are being transported that, if mixed, could react to form more hazardous compounds; safety practices (including any speed restrictions); the applicant's safety record (to the extent available) on derailments, accidents and hazardous spills; the contingency

plans to deal with accidental spills, and the likelihood of and accidental release of hazardous materials.

Abandonment will not result in transportation of hazardous materials.

(iii) If there are any known hazardous waste site or sites where there have been known hazardous materials spills on the right-of-way, identify the location of those sites and the types of hazardous materials involved.

To the best of our knowledge, there have been no hazardous material spills on the right-of-way along the proposed abandonment section. In reviewing BNSF environmental files, it was determined that a lessee along the proposed abandonment experienced an environmental incident. To our knowledge this site did not affect properties within the right-of-way. On September 6, 1994, an explosion and fire caused a release of lindane and 2,4-D (weed killer). The North Dakota Department of Health, Environmental Health Section (NDDH) required the lessee to excavate and dispose of soil and monitor ground water. In Wildrose, North Dakota (located approximately at M.P. 50, between Hamlet and Corinth) the same lessee had a gasoline release at their bulk oil facility which is adjacent to the right-of-way. Based on the investigation and remediation the NDDH closed the facility.

(8) Biological resources:

(i) Based on consultation with the U.S. Fish and Wildlife Service, state whether the proposed action is likely to adversely affect endangered or threatened species or areas designated as a critical habitat, and if so, describe the effects.

The proposed abandonment will have no adverse affect on any endangered or threatened species or areas designated as a critical habitat. See Exhibit F, response from the U.S. Fish and Wildlife Service.

(ii) State whether wildlife sanctuaries or refuges, National or State parks or forests will be affected, and describe any effects.

A portion of the right of way has been identified as a crossing through Waterfowl Production Areas administered by the U.S. Fish and Wildlife Service. See Exhibit E, letter from the U.S. Fish and Wildlife Service. The Bureau of Land Management has no wildlife sanctuaries or refuges, National or State parks or forests in the proposed project impact area. See exhibit G, letter from the Bureau of Land Management, North Dakota Field Office.

(9) Water:

(i) Based on consultation with State water quality officials, state whether the proposed action is consistent with applicable Federal, State or local water quality standards. Describe any inconsistencies.

The proposed abandonment will be consistent with applicable Federal, State and local water quality standards. The abandonment will not lead to the erosion of exposed soil surfaces and trapping sediments being transported. If the railroad runs into fragile and sensitive areas such as wetlands, riparian zones, delicate flora, or land resources, they will be protected against compaction, vegetation loss and unnecessary damage. All construction, which directly or indirectly impacts aquatic systems, will be managed to minimize impacts. All attempts will be made to prevent the

contamination of water at construction sites from fuel spillage, lubricants, and chemicals, by following safe storage and handling procedures. Stream bank and streambed disturbances will be controlled to minimize and/or prevent silt movement, nutrient upsurges, plant dislocation, and any physical, chemical, or biological disruption. The railroad will not use any fill material. See Exhibit H, letter and attached Construction and Environmental Disturbance Requirements from the North Dakota Department of Health, Environmental Health Section

(ii) Based on consultation with the U.S. Army Corps of Engineers, state whether permits under Section 404 of the Clean Water Act (33 U.S.C. § 1344) are required for the proposed action and whether any designated wetlands or 100-year floodplains will be affected. Describe the effects.

The abandonment plan will have no impacts on judicial waters of the United States, therefore a Section 404 permit is not necessary. See Exhibit I, letter from the Department of the Army Corps of Engineers, Omaha District. The BNSF railroad line between Powers Lake, North Dakota and Grenora North Dakota is not located in an identified floodplain. See Exhibit J, letter from the North Dakota State Engineer.

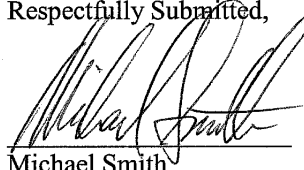
(iii) State whether permits under Section 402 of the Clean Water Act (33 U.S.C. § 1344) are required for the proposed action. (Applicants should contact the U.S. Environmental Protection Agency or the state environmental protection or equivalent agency if they are unsure whether such permits are required.)

A Section 402 permit is not required for the abandonment. See Exhibit H, letter from the North Dakota Department of Health, Environmental Health Section.

(10) **Proposed Mitigation:** Describe any actions that are proposed to mitigate adverse environmental impacts, indicating why the proposed mitigation is appropriate.

Any salvage operations that may result will be in accordance with BNSF's general practice of requiring its private contractors to comply with all federal, state and local laws and regulations pertaining to the environment, including, but not limited to noise, air quality, water quality, and items of archaeological significance. The project itself should mitigate the environmental effects of reinstating active rail operations.

Respectfully Submitted,

A handwritten signature in black ink, appearing to read "Michael Smith", is written over a horizontal line.

Michael Smith
Freeborn & Peters
311 S. Wacker Dr. Suite 3000
Chicago, Illinois 60606-6677
Phone: (312) 360-6724
Fax: (312) 360-6598

Date: June 10, 2002

BEFORE THE
SURFACE TRANSPORTATION BOARD

In the Matter of The
Burlington Northern and Santa Fe
Railway Company
Notice of Exemption to Abandon
Its Line of Railroad Between
M.P. 26.59 in Powers Lake, North Dakota
and M.P. 87.10 in Grenora, North Dakota

Docket No. AB-6
(Sub No. 394X)

HISTORICAL REPORT

The following is submitted to the Surface Transportation Board by The
Burlington Northern and Santa Fe Railway Company (BNSF) in accordance with the
Board's reporting requirements set forth in 49 C.F.R. § 1105.8 for the purpose of assisting
the Board's environmental and historical assessment regarding BNSF's Notice of
Exemption to abandon its line of railroad between M.P.26.59 in Powers Lake, Burke
County, North Dakota and M.P. 87.10 in Grenora, Williams County, North Dakota, a
total distance of 60.51 miles

(1) A U.S.G.S. topographic map (or alternate map drawn to scale and sufficiently
detailed to show buildings and other structures in the vicinity of the proposed action)
showing the location of the proposed action, and the locations and approximate
dimensions of the railroad structures that are 50 years old or older and are part of the
proposed action;

One copy of a U.S.G.S. Topographical map has been provided to the North
Dakota State Office of Historic Preservation and one copy is being provided to the
Surface Transportation Board's Section of the Environmental Analysis upon filing of this
Report.

(2) A written description of the right-of-way (including approximate widths, to the extent known) and the topography and urban and/or rural characteristics of the surrounding area:

The 60.51 mile Powers Lake to Grenora line connects the rural towns of Powers Lake and Grenora. The line is located in Burke and Williams counties of North Dakota. The adjoining land is rural in nature with much of it cultivated for wheat production. Powers Lake and Grenora are the only towns along the line with listed populations which are 309 and 202 respectively. Stations along the line include Powers Lake, Battleview, McGregor, Hamlet, Wildrose, Corinth, Alamo, Appam, Zahl, Hanks and Grenora. The town of Williston with a population of 12,512 is located approximately 40 miles south of the Powers Lake to Grenora line.

The rail line corridor is irregular in width varying from 100 to 150 feet wide. The station grounds are usually 300 feet wide.

(3) Good quality photographs (actual photographic prints, not photocopies) or railroad structures on the property that are 50 years old or older and of the immediately surrounding area;

There are no structures other than bridges on the property that are 50 years old or older. There are 28 bridges that are 50 years old or older. Pictures are provided in Exhibit K.

(4) The date(s) of construction of the structures, and the date(s) and extent of any major alterations, to the extent such information is known;

Dates of construction and dates of major alterations are provided in Exhibit K.

(5) A brief narrative history of carrier operations in the area, and an explanation of what, if any, changes are contemplated as a result of the proposed action:

The former Great Northern Railway Company (GN) acquired most of the right-of-way for the rail line corridor between Tolna and Hamar, North Dakota between 1909

and 1917. Some additional parcels were acquired between 1920 and 1940. In 1970 the GN merged with the Northern Pacific Railway, the Chicago Burlington & Quincy Railroad Company and the Spokane Portland and Seattle Railway to become the Burlington Northern Railroad Company (BN). BN subsequently merged with the Atchison Topeka and Santa Fe Railway in 1995 to become The Burlington Northern and Santa Fe Railway Company (BNSF).

The Powers Lake to Grenora line has been out of service for over two years.

(6) A brief summary of documents in the carrier's possession, such as engineering drawings, that might be useful in documenting a structure that is to be historic;

Documents in BNSF's possession concerning this abandonment may include alignment maps showing the right-of-way and/or station maps. Such documents are too large for practical reproduction in this report, but can be furnished upon request, if they are available.

(7) An opinion (based on readily available information in the railroad's possession) as to whether the site and/or structures meet the criteria for listing on the National Register of Historic Places (36 C.F.R. § 60.4), and whether there is a likelihood of archaeological resources or any other previously unknown historic properties in the project area, and the basis for these opinions (including any consultations with the State Historic Preservation Office, local historical societies or universities);

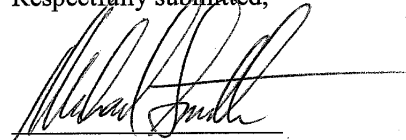
To the best of BNSF's knowledge, the proposed abandonment should have no appreciable effects on any known sites or properties listed, or eligible for listing in the National Register of Historic Places. The State Historical Society of North Dakota has determined that the no historic properties will be affected by the proposed abandonment. See Exhibit L, letter from the State Historical Society of North Dakota.

(8) A description (based on readily available information in the railroad's possession) of any known prior subsurface ground disturbance or fill, environmental conditions

(naturally occurring or manmade) that might affect the archaeological recovery of resources (such as swampy conditions or the presence of toxic wastes), and the surrounding terrain.

BNSF is not aware of any known prior subsurface ground disturbance or fill or any other environmental conditions (naturally occurring or man-made) that might affect the recovery of archaeological resources.

Respectfully submitted,

A handwritten signature in black ink, appearing to read "Michael Smith", is written over a horizontal line.

Michael Smith
Freeborn & Peters
311 S. Wacker Dr. Suite 3000
Chicago, Illinois 60606-6677
Phone: (312) 360-6724
Fax: (312) 360-6598

Date: June 10, 2002

CERTIFICATION PURSUANT TO 49 C.F.R.1105.7(c)

The Burlington Northern and Santa Fe Railway Company ("BNSF") by and through its authorized representative, Michael Smith, certifies that on May 10, 2002 BNSF sent copies of the foregoing Environmental and Historical Reports by first class mail to the following agencies:

Ms. Victoria Rutson
Chief, SEA
Surface Transportation Board
1925 K Street, N.W.
Washington, DC 20423-0001

Merlan Paaverud, Jr.
Historic Preservation Division
State Historic Society of North Dakota
612 East Boulevard
Bismarck, ND 58505-0830

Douglas Burger
Field Manager
Bureau of Land Management
U.S. Department of the Interior
2933 Third Ave. West
Dickinson, ND 58601-2619

Thomas Jewett
State Conservationist
USDA Natural Resources Conservation
Service
P.O. Box 1458
Bismarck, ND 58501

Jeff Klein
Office of the State Engineer
900 East Boulevard
Bismarck, North Dakota 58505-0850

Jesse Hanson
North Dakota Parks and Recreation
1835 Bismarck Expressway
Bismarck, ND 58504

Burke County Board of Commissioners
103 Main St. SW
Bowbells, ND 58721

Williams County Board of Commissioners
P.O. Box 20247
Williston, ND 58802

Jim Winters
State Program Manager
U.S. Army Corps of Engineers
Bismarck Regulatory Office
1513 South 12th Street
Bismarck, ND 58504

Allyn Sapa
U.S. Fish and Wildlife Service
3425 Miriam Ave.
Bismarck, ND 58501

Scott Radig
North Dakota Department of Health
Water Quality Division
P.O. Box 5520
Bismarck, ND 58506

U. S. Environmental Protection Agency
Region 8
999 18th St., Suite 300
Denver, CO 80202-2466

North Dakota Department of Health
Environmental Health Section
1200 Missouri Ave.
P.O. Box 5520
Bismarck, N.D. 58506-5520

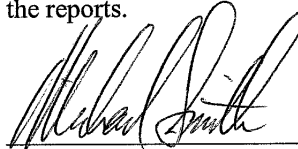
North Dakota Dept. of Transportation
608 East Boulevard Ave.
Bismarck, ND 58505-0700

North Dakota Public Service Commission
600 East Boulevard, Dept. 408
Bismarck, ND 58505-0480

U.S. Department of the Interior
National Park Service
Land Resource Division
1849 C Street, N.W.
Washington, DC 20240

Edward McKay
National Geodetic Survey
NOAA - SSMC3
1315 East-West Highway
Silver Spring, MD 20914

The required cover letters (pursuant to 49 C.F.R. 1105.11) are attached hereto. BNSF has consulted with all appropriate agencies in preparing the reports.



Michael Smith
Freeborn & Peters
311 S. Wacker Dr. Suite 3000
Chicago, Illinois 60606-6677
Phone: (312) 360-6724
Fax: (312) 360-6598

Date: June 10, 2002

A

**MAP WILL BE SCANNED
AT A LATER DATE**

B

Freeborn & Peters

August 10, 2001

Burke County Board of Commissioners
103 Main St SW
Bowbells, ND 58721

**Re: The Burlington Northern and Santa Fe Railway Company Abandonment
between Powers Lake to Grenora, North Dakota**

Attorneys at Law

311 South Wacker Drive
Suite 3000
Chicago, Illinois
60606-6677
Tel 312.360.6000

Brian Nettles
Paralegal
Direct 312.360.6336
Fax 312.360.6596
bnettl@freebornpeters.com

Chicago

Springfield

Dear Sir or Madam:

The Burlington Northern and Santa Fe Railway Company ("BNSF") plans to file an exemption to abandon its line of railroad from Powers Lake (Milepost 26.59) to Grenora (Milepost 87.10), North Dakota, a distance of 60.51 miles in the near future.

As part of the environmental report BNSF needs to know whether or not the proposed action is consistent with local land use plans and if there are any alternate public uses for the corridor such as a recreational trail.

The proposed abandonment may require the removal of the track materials such as the rails and ties but the roadbed will be left intact.

For your reference I have enclosed a map of the above referenced railroad line. Please provide this information by September 15, 2001. If you have any questions, or if you would like to discuss this matter further, please do not hesitate to contact me at (312) 360-6336.

Thank you in advance for your cooperation.

Sincerely,



Brian Nettles

/bn
Enclosure

C

Freeborn & Peters

August 10, 2001

Williams County Board of Commissioners
P.O. Box 2047
Williston, North Dakota 58802

**Re: *The Burlington Northern and Santa Fe Railway Company Abandonment
 between Powers Lake to Grenora, North Dakota***

Attorneys at Law

311 South Wacker Drive
Suite 3000
Chicago, Illinois
60606-6677
Tel 312.360.6000

Brian Nettles
Paralegal
Direct 312.360.6336
Fax 312.360.6596
bnettl@
freebornpeters.com

Chicago

Springfield

Dear Sir or Madam:

The Burlington Northern and Santa Fe Railway Company ("BNSF") plans to file an exemption to abandon its line of railroad from Powers Lake (Milepost 26.59) to Grenora (Milepost 87.10), North Dakota, a distance of 60.51 miles in the near future.

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Thank you in advance for your cooperation.

Sincerely,



Brian Nettles

/bn
Enclosure

D

Mary

Freeborn & Peters

August 9, 2001

Thomas Jewett
State Conservationist
P.O. Box 1458
Bismarck, North Dakota 58501

Attorneys at Law

311 South Wacker Drive
Suite 3000
Chicago, Illinois
60606-6677
Tel 312.360.6000

Brian Nettles
Paralegal
Direct 312.360.6336
Fax 312.360.6596
bnettl@
freebornpeters.com

**Re: The Burlington Northern and Santa Fe Railway Company Abandonment
between Powers Lake and Grenora, North Dakota**

Dear Mr. Jewett:

The Burlington Northern and Santa Fe Railway Company ("BNSF") plans to file an exemption to abandon its line of railroad from Powers Lake (Milepost 26.59) to Grenora (Milepost 87.10), North Dakota, a distance of 60.51 miles in the near future

As part of the environmental report BNSF needs to know whether or not the proposed abandonment will have any effect on prime agricultural lands.

The proposed abandonment may require the removal of the track materials such as the rails and ties but the roadbed will be left intact.

Please provide this information by September 15, 2001. Thank you for your cooperation.

For your reference I have enclosed a map of the above referenced railroad line. If you have any questions, or if you would like to discuss this matter further, please do not hesitate to contact me at (312) 360-6336.

Thank you in advance for your cooperation.

Sincerely,

Brian Nettles

Brian Nettles

/bn

Enclosure

USDA
NATURAL RESOURCES
CONSERVATION SERVICE

Project as described will have NO EFFECT on
prime farmlands or wetlands on agricultural
lands. IF PROJECT DESIGN CHANGES ARE
MADE, PLEASE SUBMIT PLANS FOR
REVIEW.

State Conservationist

Date

8-15-01

E



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Wetland Acquisition Office
3425 Miriam Avenue
Bismarck, North Dakota 58501



September 14, 2001

Brian Nettles
Freeborn and Peters, Attorneys at Law
311 South Wacker Drive, Suite 3000
Chicago, Illinois 60606-6677

RE: The Burlington Northern and Santa Fe Railway Company Abandonment
Between Powers Lake and Grenora, North Dakota

Dear Mr. Nettles:

After a review of the proposed abandonment of 60.51 miles of railroad right-of-way from Powers Lake, North Dakota to Grenora, North Dakota, a portion of the right-of-way has been identified as crossing through Waterfowl Production Areas administered by the U.S. Fish and Wildlife Service (Service).

The Service is very interested in purchasing those portions of the right-of-way lying within the following legal descriptions:

Burke County, North Dakota
T. 159 N., R. 93 W., 5th P.M.
Section 27, Lot 6, NE $\frac{1}{4}$ SE $\frac{1}{4}$ SE $\frac{1}{4}$

Williams County, North Dakota
T. 159 N., R. 100 W., 5th P.M.
Section 29, SE $\frac{1}{4}$ NW $\frac{1}{4}$, SW $\frac{1}{4}$ NE $\frac{1}{4}$
Section 30 Lot 2, SE $\frac{1}{4}$ NW $\frac{1}{4}$

T. 159 N., R. 101 W., 5th P.M.
Section 25, S $\frac{1}{2}$ NE $\frac{1}{4}$

If you have any questions, please contact me at our Bismarck, North Dakota office,
(701) 250-4415, Monday through Friday, between the hours of 8:00 am and 4:30 pm CST.

Sincerely,

Stuart A. Wacker
Realty Field Supervisor

cc: Project Leader, Des Lacs NWR
Field Supervisor, Ecological Services, North Dakota Field Office

F

Freeborn & Peters

August 14, 2001

Allyn Sapa
U. S. Fish and Wildlife Service
3425 Miriam Avenue
Bismarck, North Dakota 58501

Attorneys at Law

311 South Wacker Drive
Suite 3000
Chicago, Illinois
60606-6677
Tel 312.360.6000

Brian Nettles
Litigation Clerk
Direct 312.360.6336
Fax 312.360.6596
bnettl@freebornpeters.com

Re: **The Burlington Northern and Santa Fe Railway Company Abandonment
between Tolna and Hamar, North Dakota**

Dear Mr. Sapa:

On May 7, 2001 I wrote you a letter regarding the Burlington Northern and Santa Fe Railway Company's ("BNSF") plans to file an exemption to abandon its line of railroad from Tolna (Milepost 92.30) to Hamar (Milepost 98.00), North Dakota, a distance of 5.70 miles. In the past couple of days, Burlington Northern and Santa Fe has decided to extend the abandonment to Milepost 92.00, rather than 92.30 for a new distance of 6.00 miles.

If there are any changes in your opinion due the extension of the abandonment, please provide comments by September 1, 2001. For your reference I have enclosed a copy of my original letter, your response and a copy of a map of the above referenced railroad line. If you have any questions, or if you would like to discuss this matter further, please do not hesitate to contact me at (312) 360-6336.

Thank you in advance for your cooperation.

Sincerely,



Brian Nettles

U.S. FISH AND WILDLIFE SERVICE
ECOLOGICAL SERVICES
ND FIELD OFFICE

/bn
Enclosure

Project as described will have no significant impact on fish and wildlife resources. No endangered or threatened species are known to occupy the project area. IF PROJECT DESIGN CHANGES ARE MADE, PLEASE SUBMIT PLANS FOR REVIEW.

9/5/01

Date

Allyn J. Sapa
Field Supervisor

F



IN REPLY TO:

United States Department of the Interior

BUREAU OF LAND MANAGEMENT

North Dakota Field Office

2933 Third Avenue West

Dickinson, North Dakota 58601-2619

701-227-7700

2700

LRG

September 11, 2001

Brian Nettles, Litigation Clerk
Freeborn & Peters
311 South Wacker Drive, Suite 3000
Chicago, IL 60606-6677

Re: The Burlington Northern and Santa Fe Railway Company Abandonment
Between Powers Lake and Grenora, North Dakota

Dear Mr. Nettles:

The Bureau of Land Management has no wildlife sanctuaries or refuges, National or State parks or forests in the proposed project impact area.

However, please keep the Bureau informed as to the continued processes. These railroad abandonments, depending under which Act they were granted, affect the Bureau. We appreciate the opportunity to comment.

Sincerely,

Linda Gisvold
Realty

G

H



NORTH DAKOTA DEPARTMENT OF HEALTH
Environmental Health Section

Location:

1200 Missouri Avenue
Bismarck, ND 58504-5264

Fax #:

701-328-5200

Mailing Address:

P.O. Box 5520
Bismarck, ND 58506-5520

August 16, 2001

Brian Nettles
Freeborn & Peters
311 S Wacker Dr., Ste. 3000
Chicago, IL 60606-6677

Re: Burlington Northern - Santa Fe Railway Company Line
Abandonment Between Powers Lake and Grenora, ND

Dear Mr. Nettles:

This letter is in response to your questions regarding compliance with water quality standards, and other requirements as a result of the proposed line abandonment. The attached sheet outlines the minimum construction and environmental disturbance requirements of the North Dakota Department of Health. A Section 402 permit is not required for this project, but please check with the US Army Corps of Engineers for their requirements on Section 404 permits.

The Department also requests that a Phase I/II environmental audit be completed for abandoned railroad property or lease hold property in communities where activities may have taken place that may have impacted soils or groundwater.

If you have any questions, please call me at 701-328-5233.

Sincerely,

Scott Radig
Environmental Engineer
Div. Of Water Quality

SR:dgg

xc: Mike Sauer, Div. Of Water Quality

L. David Glatt, Div. Of Waste Management

Environmental Health
Section Chief's Office
701-328-5150

Air
Quality
701-328-5188

Municipal
Facilities
701-328-5211

Waste
Management
701-328-5166

Water
Quality
701-328-5210

Website: www.health.state.nd.us/ndhd/envir
Printed on recycled paper.



NORTH DAKOTA
DEPARTMENT OF HEALTH

ENVIRONMENTAL HEALTH SECTION

1200 Missouri Avenue
P.O. Box 5520
Bismarck, North Dakota 58506-5520
Fax #701-328-5200

Construction and Environmental Disturbance Requirements

These represent the minimum requirements of the North Dakota Department of Health. They ensure that minimal environmental degradation occurs as a result of construction or related work which has the potential to affect the waters of the State of North Dakota. All projects will be designed and implemented to restrict the losses or disturbances of soil, vegetative cover, and pollutants (chemical or biological) from a site.

Soils

Prevent the erosion of exposed soil surfaces and trapping sediments being transported. Examples include, but are not restricted to, sediment dams or berms, diversion dikes, hay bales as erosion checks, riprap, mesh or burlap blankets to hold soil during construction, and immediately establishing vegetative cover on disturbed areas after construction is completed. Fragile and sensitive areas such as wetlands, riparian zones, delicate flora, or land resources will be protected against compaction, vegetation loss, and unnecessary damage.

Surface Waters

All construction which directly or indirectly impacts aquatic systems will be managed to minimize impacts. All attempts will be made to prevent the contamination of water at construction sites from fuel spillage, lubricants, and chemicals, by following safe storage and handling procedures. Stream bank and stream bed disturbances will be controlled to minimize and/or prevent silt movement, nutrient upsurges, plant dislocation, and any physical, chemical, or biological disruption. The use of pesticides or herbicides in or near these systems is forbidden without approval from this Department.

Fill Material

Any fill material placed below the high water mark must be free of top soils, decomposable materials, and persistent synthetic organic compounds (in toxic concentrations). This includes, but is not limited to, asphalt, tires, treated lumber, and construction debris. The Department may require testing of fill materials. All temporary fills must be removed. Debris and solid wastes will be removed from the site and the impacted areas restored as nearly as possible to the original condition.

Environmental Health Section
and Enforcement
701-328-5150

Environmental
Engineering
701-328-5188

Municipal
Facilities
701-328-5211

Waste
Management
701-328-5166

Water
Quality
701-328-5210

Printed on recycled paper.

1



DEPARTMENT OF THE ARMY
CORPS OF ENGINEERS, OMAHA DISTRICT
NORTH DAKOTA REGULATORY OFFICE
1513 S. 12TH STREET
BISMARCK, NORTH DAKOTA 58504-6640

REPLY TO
ATTENTION OF

August 24, 2001

North Dakota Regulatory Office

[200160336]

Mr. Brian Nettles
Freeborn & Peters
311 South Wacker Drive
Suite 3000
Chicago, Illinois 60606-6677

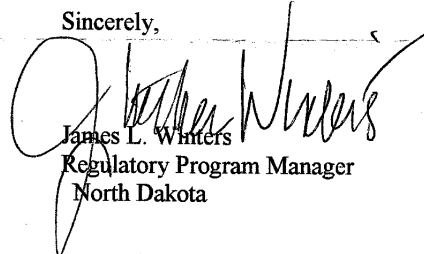
Dear Mr. Nettles:

This is in reference to your request on behalf of the **Burlington Northern and Santa Fe Railway Company** for a Department of the Army (DA) permit under Section 404 of the Clean Water Act. The Burlington Northern and Santa Fe Railway Company plans to abandon its railroad line from Powers Lake, North Dakota (Milepost 26.59) to Grenora, North Dakota (Milepost 87.10), a distance of 60.51 miles. The abandonment may require the removal of the track materials such as the rails and ties but the roadbed will be left intact. The legal description is numerous sections in Township 159 North, Ranges 95, 96, 97, 98, 99, 100, 101, 102 and 103 West, Williams County and numerous sections in Township 159 North, Range 93 and 94 West, Burke County, North Dakota.

Based on the information you provided, it has been determined that the Burlington Northern and Santa Fe Railway Company proposed abandonment plan will have no impacts on jurisdictional waters of the United States. However, DA authorization may be required should changes be made to the Burlington Northern and Santa Fe Railway abandonment plan.

If you have any questions concerning this determination, please do not hesitate to contact this office by letter or telephone (701) 255-0015 and reference project number 200160336.

Sincerely,



James L. Winters
Regulatory Program Manager
North Dakota

J



Office of the State Engineer

August 14, 2001

Brian Nettles
Freeborn & Peters
311 South Wacker Drive
Suite 3000
Chicago, IL 60606-6677

**Re: The Burlington Northern and Santa Fe Railway Company
Abandonment between Powers Lake and Grenora, North
Dakota**

Dear Mr. Nettles:

The BNSF railroad line between Powers Lake, ND and Grenora, ND is not located in an identified floodplain. No floodplain maps exist for this area.

Feel free to call, if you have questions.

Sincerely,

Bruce Lange
Planner II

FOR THE RECORD, THE BNSF RAILROAD LINE

located in an identified floodplain. No floodplain maps exist for this area.
The BNSF railroad line between Powers Lake, ND and Grenora, ND is not

Dear Mr. Nettles:

K

MONTANA DIVIN.

GRENORA SUB.

L/S 265

BR. # 27.1 –85.0

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 27.1

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	27.100	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	70	Alpha Layout:	
Asset ID:	0000000000006236	Year Built:	1945	Height (ft.):	8
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	S-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:		DRAIN	
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	70	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 31.2

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	31.200	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006237	Year Built:	1944	Height (ft.):	12
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 31.9

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	31.900	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006238	Year Built:	1945	Height (ft.):	13
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 32.7

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	32.700	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006239	Year Built:	1943	Height (ft.):	12
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Constant radius curve
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

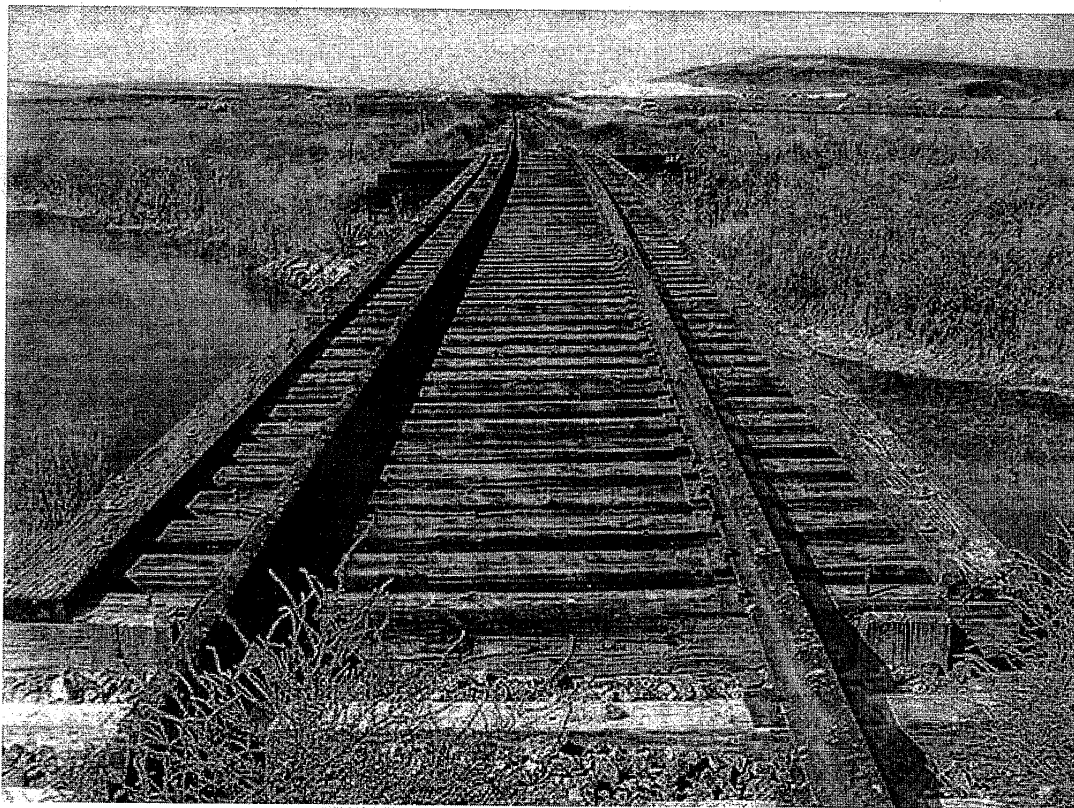
Timber Inspection Details			
Contract Insp. Date:		Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):

Hydrologic Details

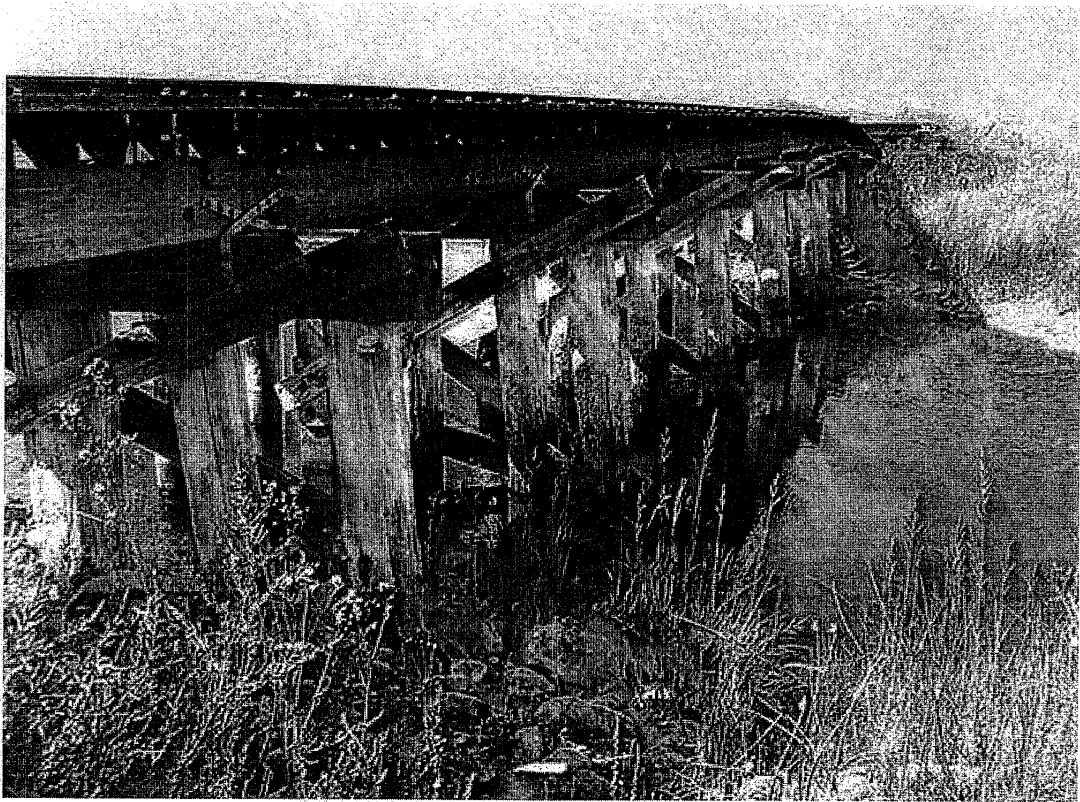
Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

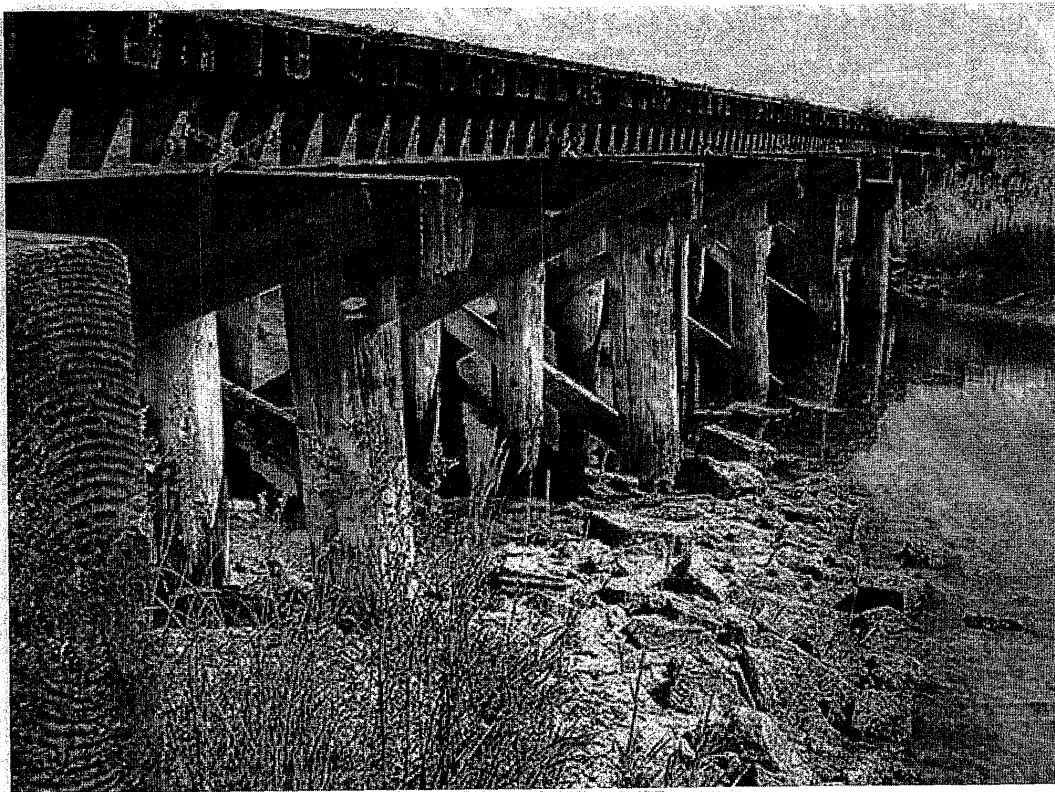
* Indicates that the specified field must be filled in before saving.



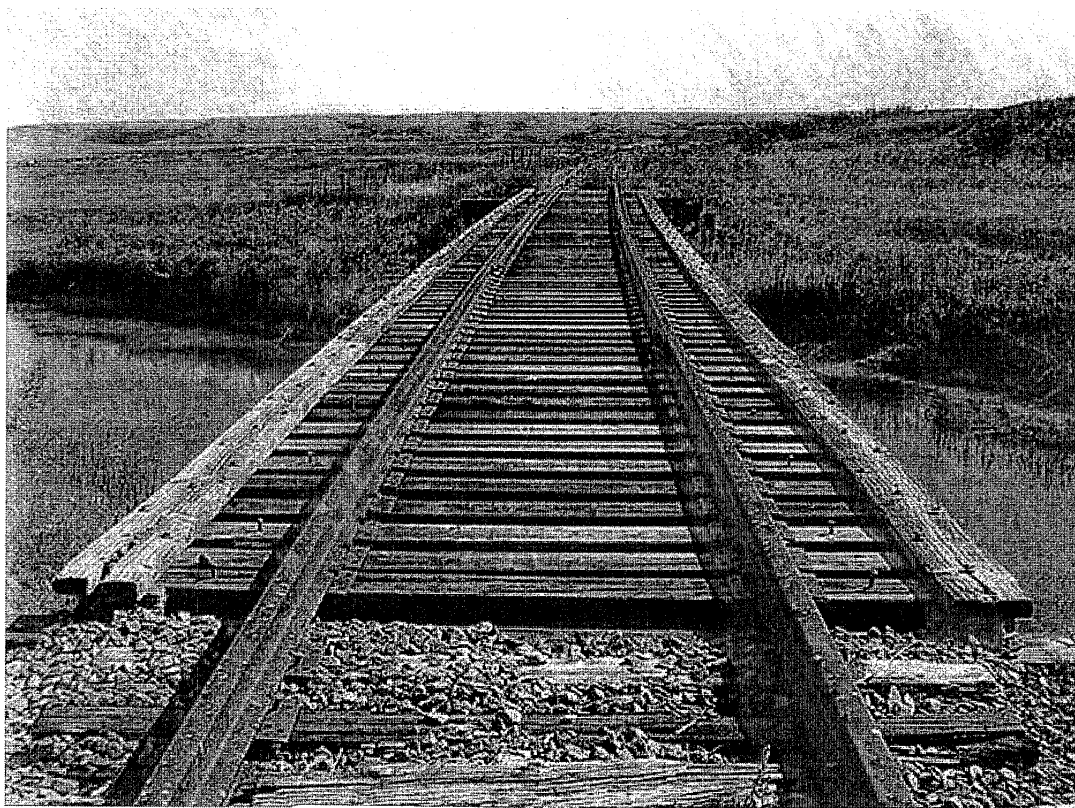
Mont. Driv., Grenora sub.
HS 265
BR. # 327
TOP- Looking West



MONT. DIVIN., GRENORA SUB
HS 265
BR. #32.7
Side - Looking South



Plant. Div'n., Gen'l. Sub
HS 265
BR. #327
Side-Looking North



MONT. DIVIN., Grenora Sub.
L/S 265
BR # 3207
TOP - LOOKING EAST.

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 33.5

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	33.500	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006240	Year Built:	1944	Height (ft.):	9
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details		
Water Presence:		Drain (Acres): 0
Scour Protection:		Scour Prot. Location:
Scour Prot. Remarks:		

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 34.3

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	34.300	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006241	Year Built:	1945	Height (ft.):	8
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:			Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 35.1

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	35.100	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	98	Alpha Layout:	
Asset ID:	0000000000006242	Year Built:	1945	Height (ft.):	14
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:			
USGS Elev. Marker:		Detector Type(s):			
RR Clear Control:		Vertical Road Clear:			
Slow Order MPH:		Slow Order Reason:			
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	98	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

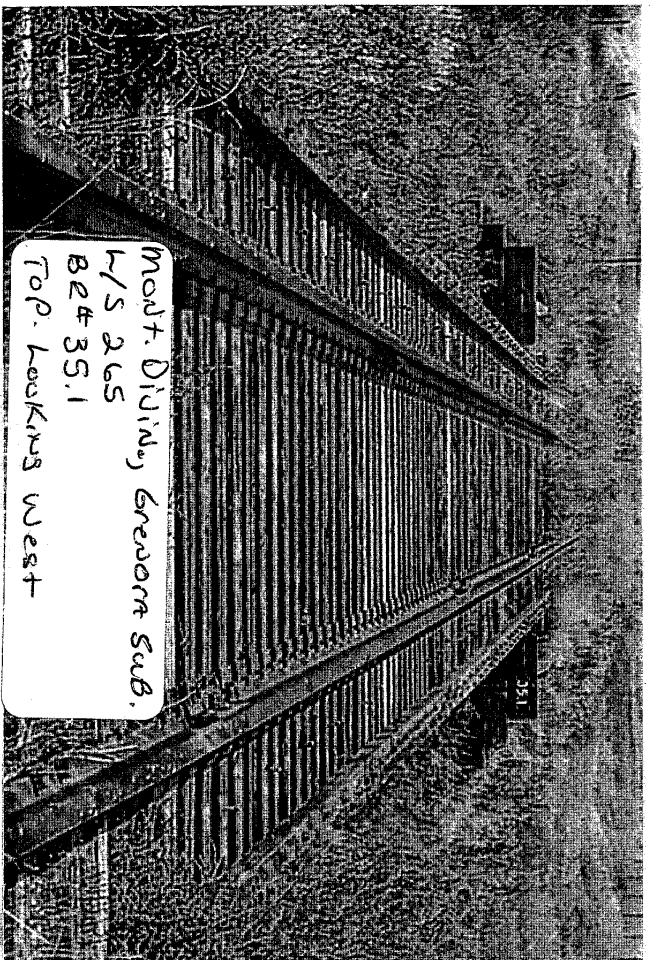
Timber Inspection Details				
Contract Insp. Date:			Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

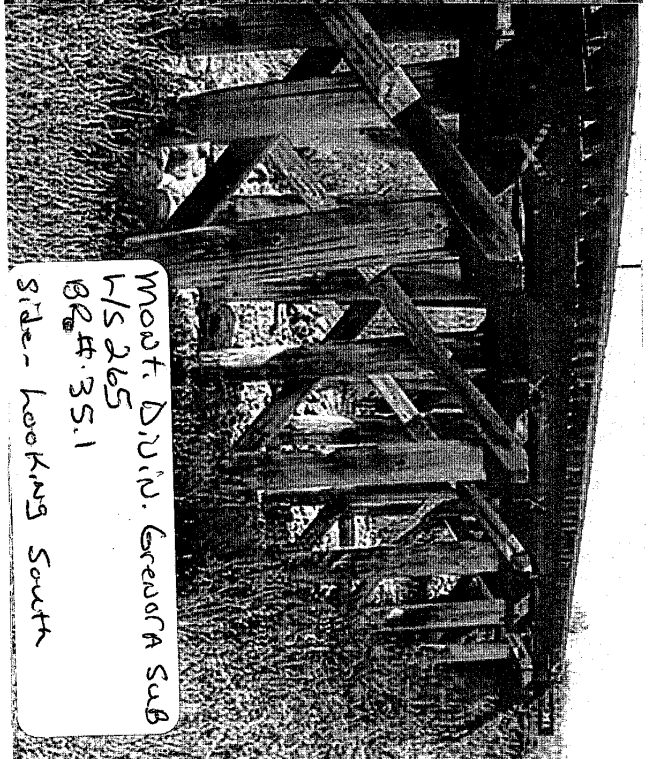
Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

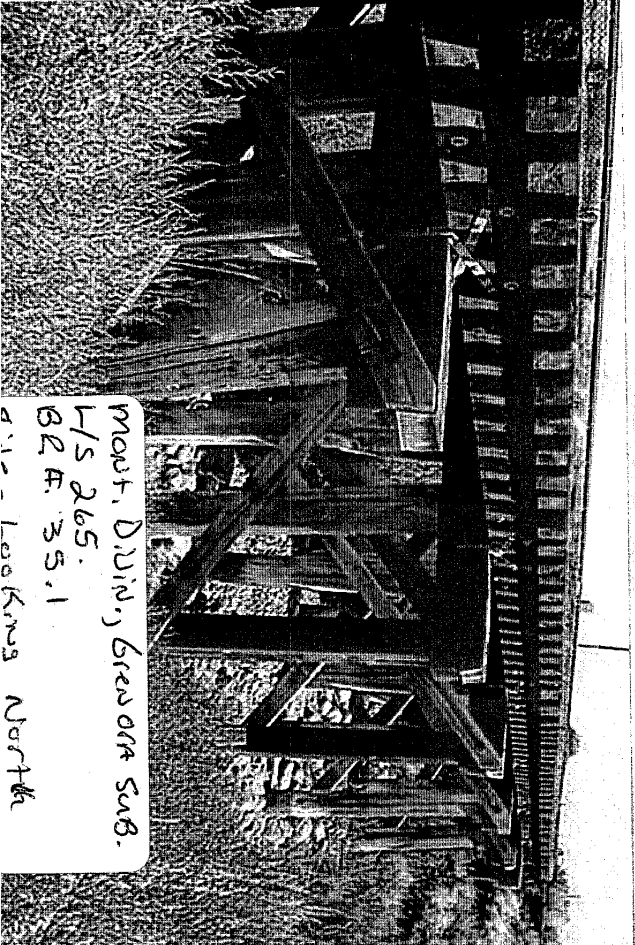
* Indicates that the specified field must be filled in before saving.



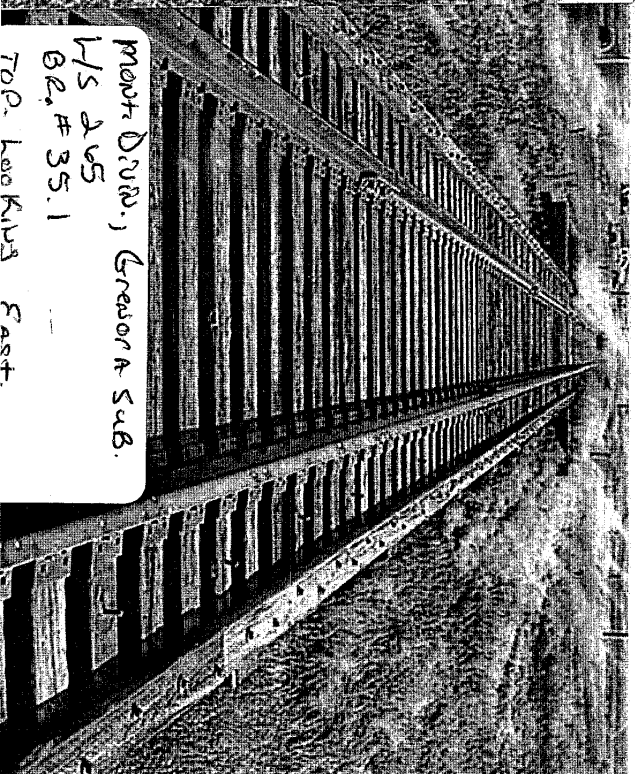
Mount. Divin, Greenock Sub.
1/5 265
BE# 35.1
Top. Looking West



Mount. Divin, Greenock Sub
1/5 265
BE# 35.1
Side- Looking South



Mount. Divin, Greenock Sub.
1/5 265
BE# 35.1
Top. Looking North



Mount. Divin, Greenock Sub.
1/5 265
BE# 35.1
Top. Looking East

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 35.5

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	35.500	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	97	Alpha Layout:	
Asset ID:	0000000000006243	Year Built:	1967	Height (ft.):	19
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	97	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 36.4

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	36.400	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	86	Alpha Layout:	
Asset ID:	0000000000006244	Year Built:	1998	Height (ft.):	18
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	Walk right side
*Description:	-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	POWERS LAKE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	86	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 39.3

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	39.300	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:	A	Alpha ID:	
*Use:	Railroad	*Length (ft.):	69	Alpha Layout:	
Asset ID:	0000000000006245	Year Built:	1991	Height (ft.):	11
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	Walk left side
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	MCGREGOR	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:			
USGS Elev. Marker:		Detector Type(s):		*Inner Guard Rail:	No inner guard rail
RR Clear Control:		Vertical Road Clear:		Site Access	Highway Vehicle Access
Slow Order MPH:		Slow Order Reason:		Vertical Rd. Clear Sign:	
Builder Type:		Bridge Fabricator:		Bridge Alignment:	Tangent
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	69	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:	PILING WAS REDRIVEN AND DECK SHIFTED IN SUMMER OF 1991.				

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

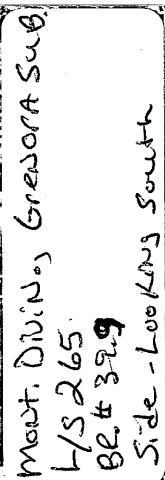
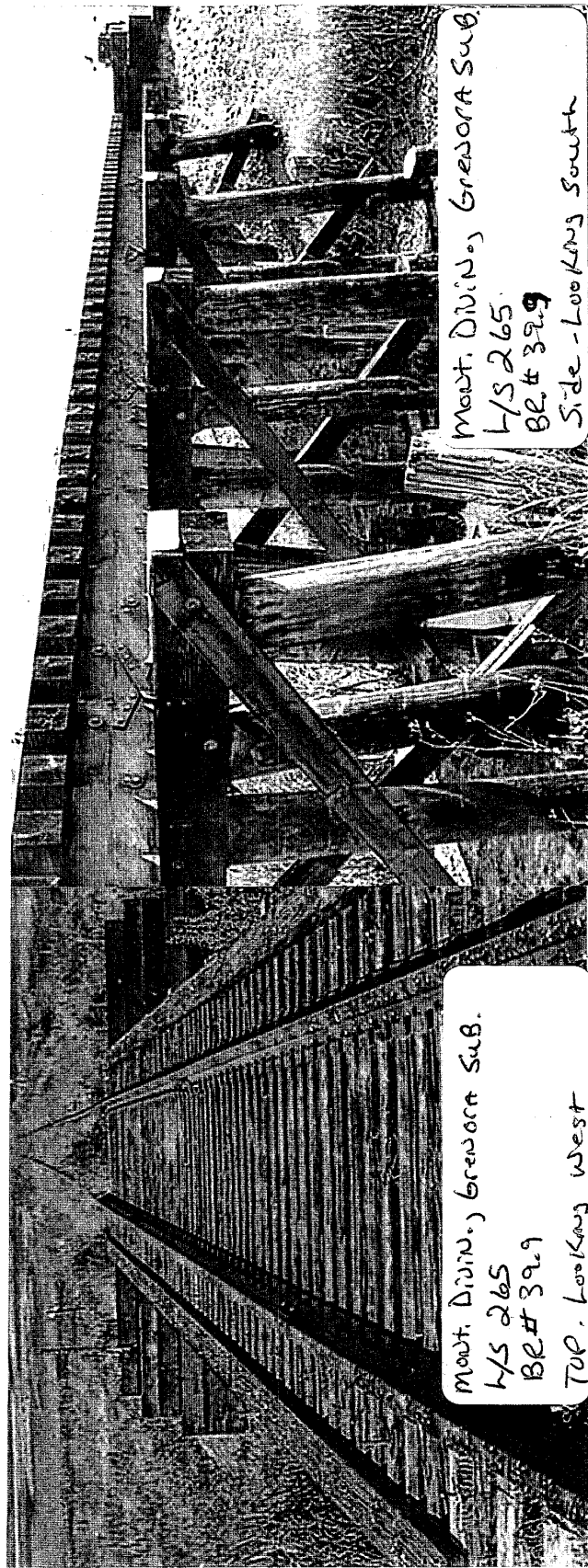
Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 39.9



Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 41.0

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	41.000	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	70	Alpha Layout:	
Asset ID:	0000000000006247	Year Built:	1969	Height (ft.):	17
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	MCGREGOR	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	70	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:		Treatment Date:			
Est. Timber Decay (%)		Est. Timber Except. (%)		Est. Grd. Line (%)	
Act. Timber Decay (%)		Act. Timber Except. (%)		Act. Grd. Line (%)	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 56.4

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	56.400	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	56	Alpha Layout:	
Asset ID:	0000000000006248	Year Built:	1947	Height (ft.):	8
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	4-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	WILDROSE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	56	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 58.5

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	58.500	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	55	Alpha Layout:	
Asset ID:	0000000000006249	Year Built:	1945	Height (ft.):	7
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	4-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	WILDROSE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	55	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 58.8

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	58.800	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	97	Alpha Layout:	
Asset ID:	0000000000006250	Year Built:	1945	Height (ft.):	10
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	7-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	WILDROSE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Constant radius curve
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	97	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 59.1

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	59.100	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	70	Alpha Layout:	
Asset ID:	0000000000006251	Year Built:	1945	Height (ft.):	11
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	WILDROSE	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	70	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 61.2

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	61.200	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	54	Alpha Layout:	
Asset ID:	0000000000006252	Year Built:	1945	Height (ft.):	8
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	4-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	WILDROSE	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:			
USGS Elev. Marker:		Detector Type(s):		*Inner Guard Rail:	No inner guard rail
RR Clear Control:		Vertical Road Clear:		Site Access:	Highway Vehicle Access
Slow Order MPH:		Slow Order Reason:		Vertical Rd. Clear Sign:	
Builder Type:		Bridge Fabricator:		Bridge Alignment:	Tangent
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	54	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:			Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 73.4

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	73.400	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	56	Alpha Layout:	
Asset ID:	0000000000006253	Year Built:	1947	Height (ft.):	7
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	4-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	APPAM	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Rail Access Only
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	56	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details			
Contract Insp. Date:		Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details	
Water Presence:	Drain (Acres): 0
Scour Protection:	Scour Prot. Location:
Scour Prot. Remarks:	

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 73.6

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	73.600	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	42	Alpha Layout:	
Asset ID:	0000000000006254	Year Built:	1947	Height (ft.):	5
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	3-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	APPAM	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Rail Access Only
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	42	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details	
Water Presence:	Drain (Acres): 0
Scour Protection:	Scour Prot. Location:
Scour Prot. Remarks:	

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 74.2

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	74.200	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	56	Alpha Layout:	
Asset ID:	0000000000006255	Year Built:	1947	Height (ft.):	9
Track Type:	Mainline or Branchline	Track Number:	6	*Walkway:	Walk both sides
*Description:	4-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	APPAM	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Rail Access Only
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	56	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

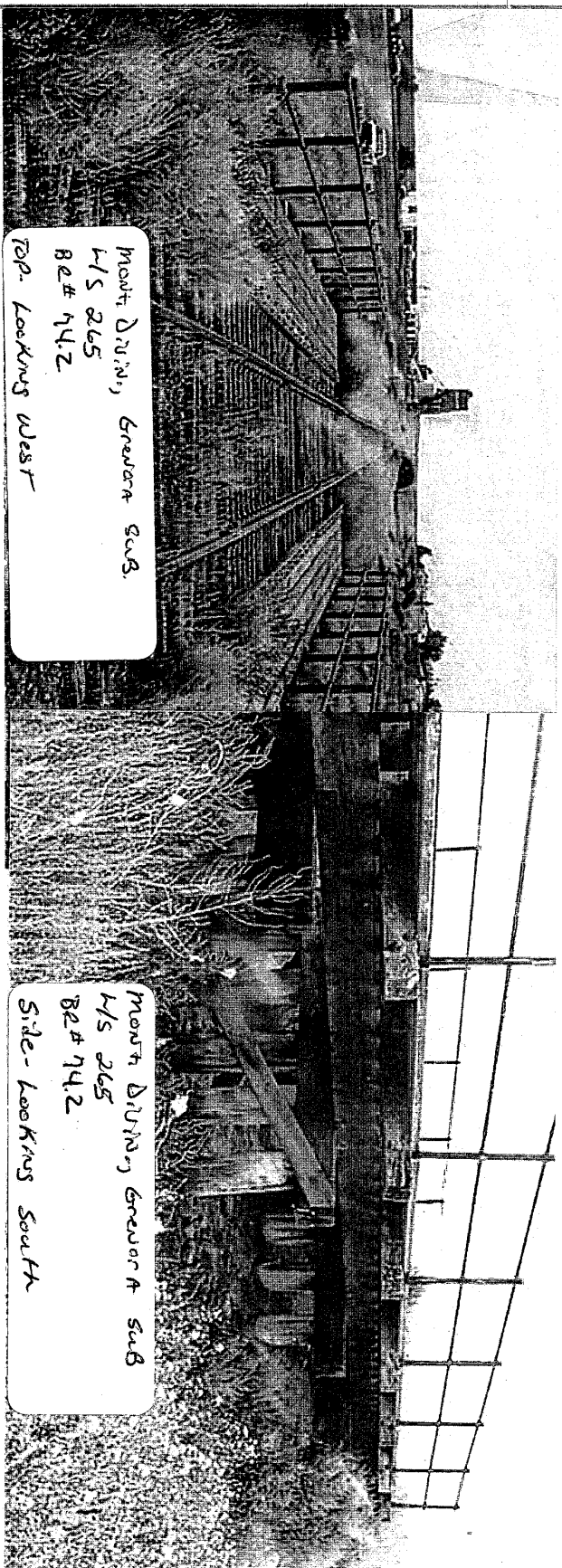
Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

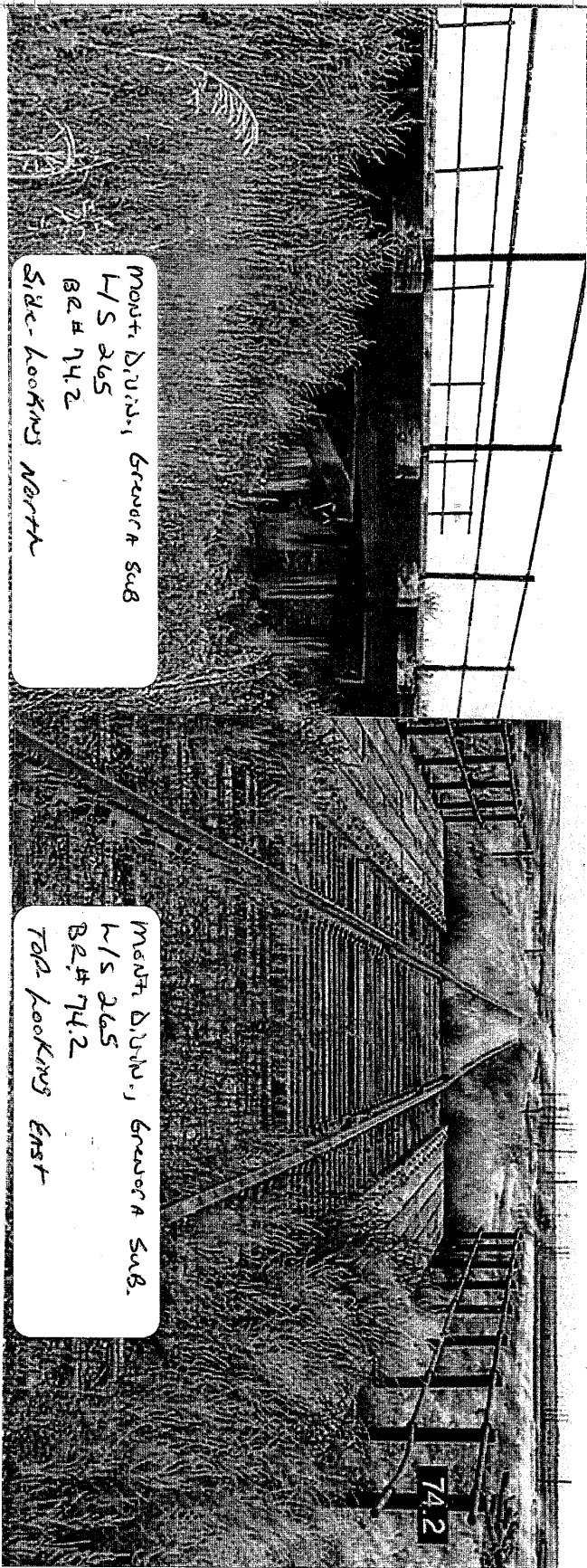
Special Condition

* Indicates that the specified field must be filled in before saving.



Month Dining, Governor Sub.
L/S 265
BE# 742
Top. Looking West

Month Dining, Governor Sub.
L/S 265
BE# 742
Side- Looking South



Month Dining, Governor Sub.
L/S 265
BE# 742
Side- Looking North

Month Dining, Governor Sub.
L/S 265
BE# 742
Top. Looking East

742

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 77.4

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	77.400	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	70	Alpha Layout:	
Asset ID:	000000000006256	Year Built:	1968	Height (ft.):	10
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	70	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 78.4

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	78.400	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	69	Alpha Layout:	
Asset ID:	0000000000006257	Year Built:	1968	Height (ft.):	10
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	69	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 79.0

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	79.000	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	68	Alpha Layout:	
Asset ID:	0000000000006258	Year Built:	1973	Height (ft.):	10
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	5-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	68	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details			
Contract Insp. Date:		Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	
		Est. Grd. Line (%):	
		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 80.0

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	80.000	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	42	Alpha Layout:	
Asset ID:	0000000000006259	Year Built:	1966	Height (ft.):	6
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	Walk left side
*Description:	3-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Constant radius curve
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	42	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%)		Est. Timber Except. (%)		Est. Grd. Line (%)	
Act. Timber Decay (%)		Act. Timber Except. (%)		Act. Grd. Line (%)	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 80.5

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	80.500	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	42	Alpha Layout:	
Asset ID:	0000000000006260	Year Built:	1966	Height (ft.):	9
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	3-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL			*Intersection:	DRAIN
Design Rate:	E58	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Constant radius curve
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	42	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details			
Contract Insp. Date:		Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):

Hydrologic Details

Hydrologic Details		
Water Presence:		Drain (Acres): 0
Scour Protection:		Scour Prot. Location:
Scour Prot. Remarks:		

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 81.2

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	81.200	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	42	Alpha Layout:	
Asset ID:	0000000000006261	Year Built:	1979	Height (ft.):	7
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	3-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	42	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details				
Contract Insp. Date:		Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):	Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details	
Water Presence:	Drain (Acres): 0
Scour Protection:	Scour Prot. Location:
Scour Prot. Remarks:	

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 83.8

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	83.800	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	28	Alpha Layout:	
Asset ID:	0000000000006262	Year Built:	1978	Height (ft.):	7
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	2-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access:	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Constant radius curve
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	28	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details					
Contract Insp. Date:			Treatment Date:		
Est. Timber Decay (%):		Est. Timber Except. (%):		Est. Grd. Line (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):		Act. Grd. Line (%):	

Hydrologic Details

Hydrologic Details			
Water Presence:		Drain (Acres):	0
Scour Protection:		Scour Prot. Location:	
Scour Prot. Remarks:			

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**

Montana Division
Grenora Sub. Divin.
L/S 265
Br.# 85.0

SIR Asset Maintenance - BRIDGE

Identification and Overview					
Line Segment:	265	Mile Post Name:	85.000	Spur Mile Post Name:	
*Asset Status:	Active (In Service)	Mile Post Suffix:		Alpha ID:	A
*Use:	Railroad	*Length (ft.):	42	Alpha Layout:	
Asset ID:	0000000000006263	Year Built:	1973	Height (ft.):	7
Track Type:	Mainline or Branchline	Track Number:	S	*Walkway:	No walk
*Description:	3-OPT				

Bridge Attributes

Bridge Attributes					
D.O.T. Number:					
Near Town:	ZAHL	*Intersection:	DRAIN		
Design Rate:	E70	Scour Rate:		*Inner Guard Rail:	No inner guard rail
USGS Elev. Marker:		Detector Type(s):		Site Access	Highway Vehicle Access
RR Clear Control:		Vertical Road Clear:		Vertical Rd. Clear Sign:	
Slow Order MPH:		Slow Order Reason:		Bridge Alignment:	Tangent
Builder Type:		Bridge Fabricator:			
Owner:	BNRR	Maint. Resp:	BNRR		
Timber Length (ft.):	42	Steel Length (ft.):	0	Concrete Length (ft.):	0
Bridge Remarks:					

Special Inspection Details

Special Inspection Details			
Underwater Insp:	<input type="radio"/> Yes <input type="radio"/> No	Underwater Insp. Date:	
Snooper Insp:	<input type="radio"/> Yes <input type="radio"/> No	Snooper Insp. Date:	

Timber Inspection Details

Timber Inspection Details			
Contract Insp. Date:		Treatment Date:	
Est. Timber Decay (%):		Est. Timber Except. (%):	
Act. Timber Decay (%):		Act. Timber Except. (%):	
		Est. Grd. Line (%):	
		Act. Grd. Line (%):	

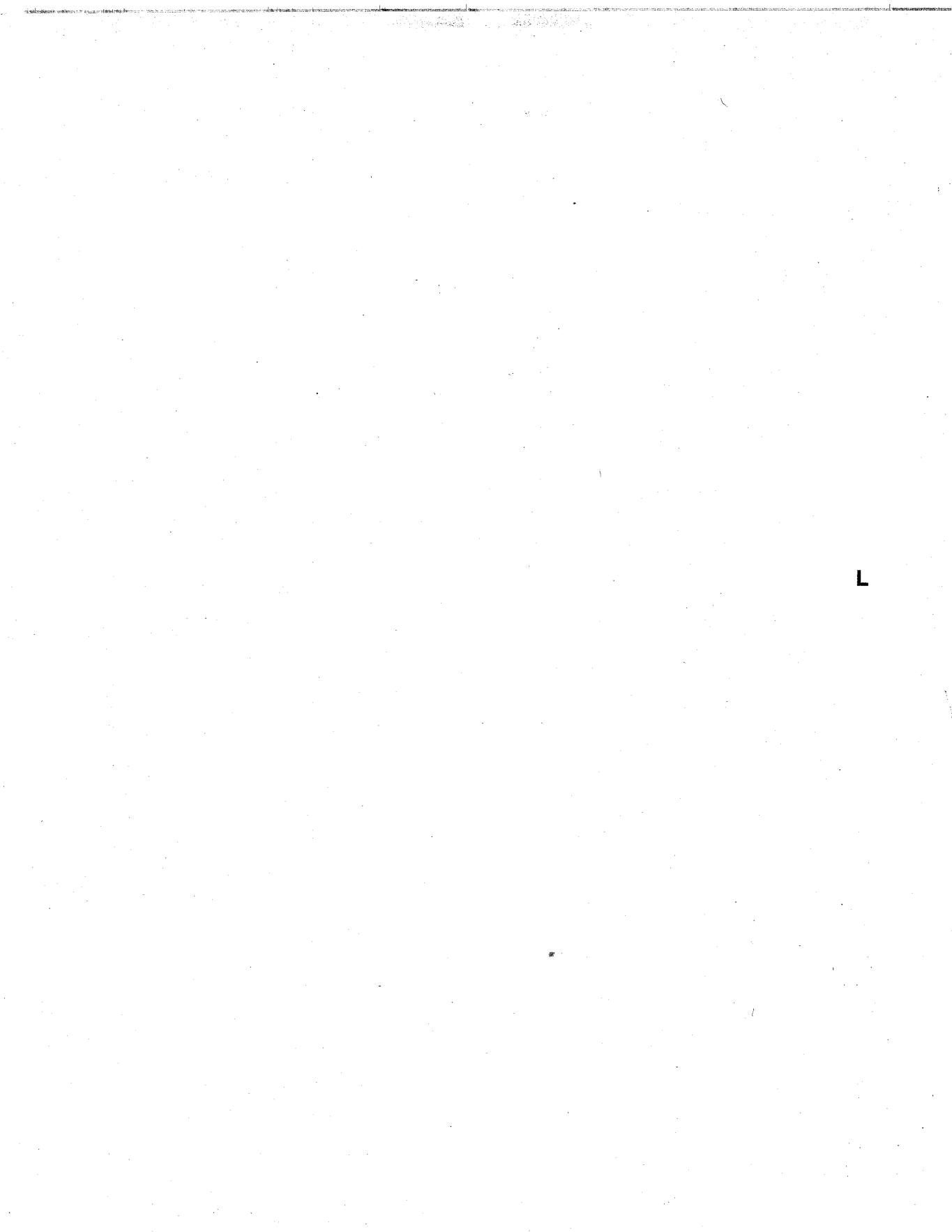
Hydrologic Details

Hydrologic Details	
Water Presence:	Drain (Acres): 0
Scour Protection:	Scour Prot. Location:
Scour Prot. Remarks:	

Special Condition

* Indicates that the specified field must be filled in before saving.

**MAP WILL BE SCANNED
AT A LATER DATE**





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January 28, 2002

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Brian Nettles
Freeborn & Peters
311 South Wacker Drive
Suite 3000
Chicago, Illinois 60606-6677

ND SHPO Ref.: 02-0085, BNSF Railway Company Abandonment Between Powers Lake and Grenora, ND.

Dear Mr. Nettles,

Mr. Steve Sand, SHSND Historical Architect, reviewed Project: 02-0085, BNSF Railway Company abandonment of 60.51 miles of rail line between the towns of Powers Lake and Grenora, ND on October 19, 2001.

We concur with "No Historic Properties Affected" determination provided the project is of the nature specified and takes place in the legal description listed and plotted in the correspondence.

We thank your department, and the BNSF Railway Company, for the opportunity to review this project. Please include the ND SHPO Reference number listed above in any further correspondence for this specific project. If you have any questions please contact Duane Kliner at (701) 328-3576.

Sincerely,

Merlan E. Paaverud, Jr.
State Historic Preservation Officer
(North Dakota)

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